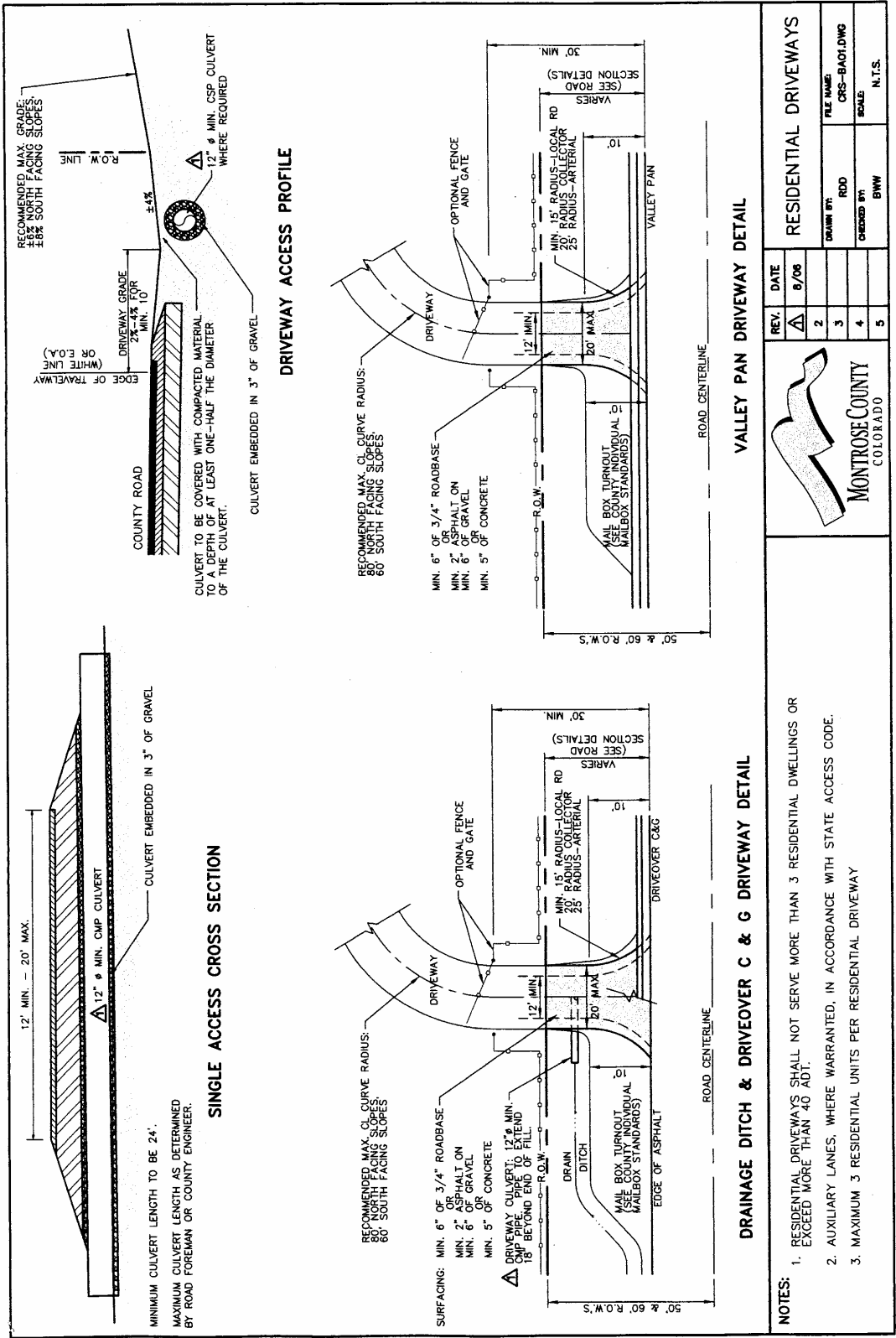
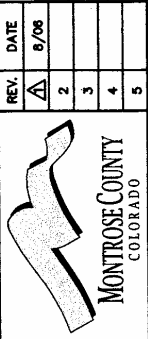


APPENDIX F – MISCELLANEOUS STANDARDS

- COMMERCIAL/INDUSTRIAL DRIVEWAYS (Revised 8/06)
- RESIDENTIAL DRIVEWAYS (Revised 8/06)
- FIELD APPROACH DRIVEWAYS (Revised 8/06)
- NEW INDIVIDUAL MAILBOX DETAILS
- MULTIPLE BOX UNIT TURN OUT DETAIL (Revised 8/06)
- CURB AND GUTTER DETAILS
- UTILITY LOCATION AND EASEMENT GUIDELINES FOR ALL STREETS
- ROLLING DIP DETAILS
- SUBDIVISION SIGN PROGRAM – Resolution # 74-2005

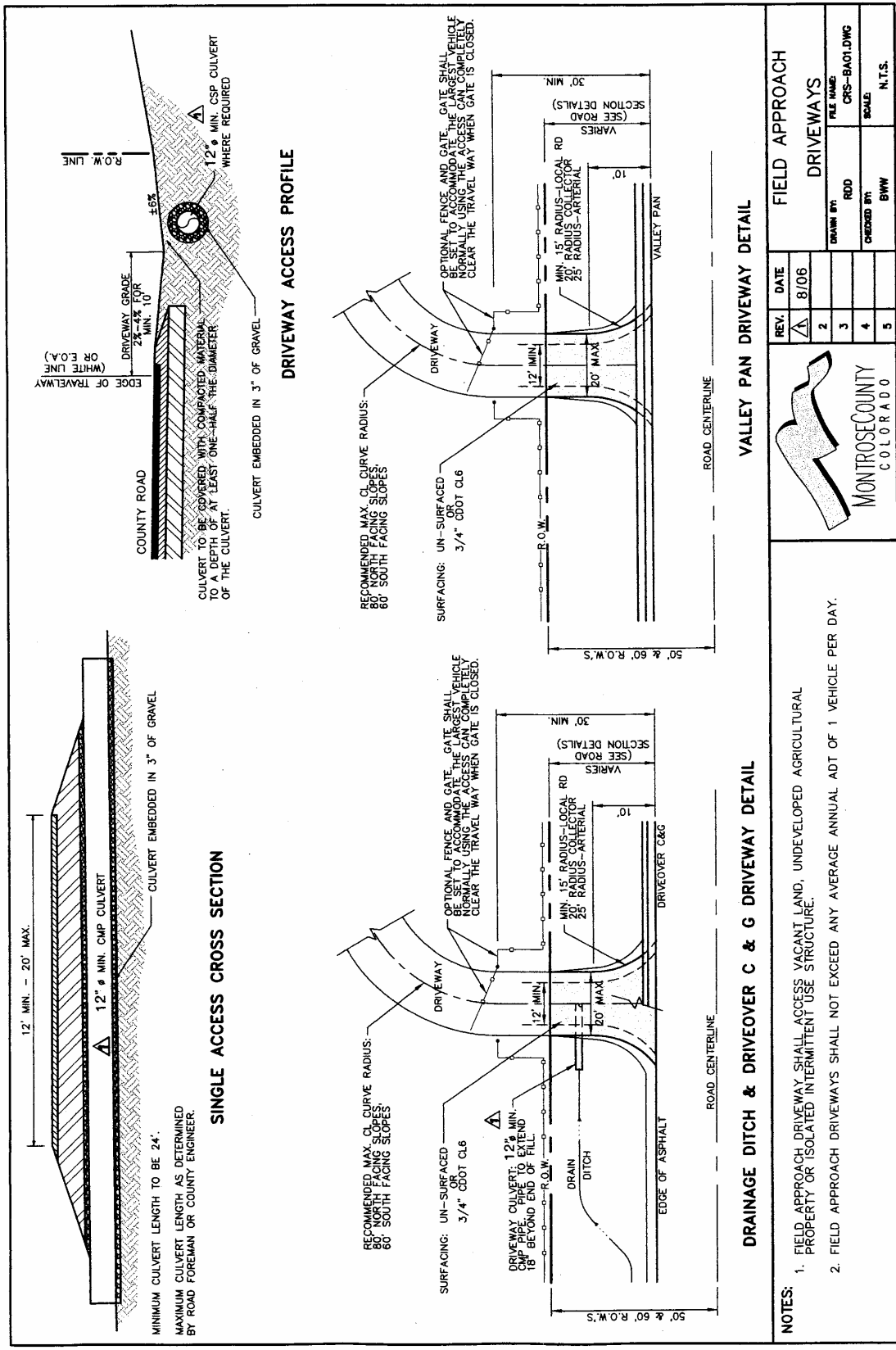


REV.	DATE	DESCRIPTION
1	8/08	INITIAL
2		
3		
4		
5		

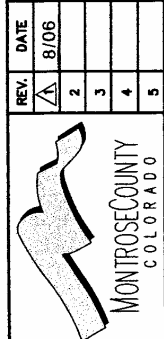


FILE NAME:	CRS-BA01.DWG
DRAWN BY:	ROD
CHECKED BY:	BWW
SCALE:	N.T.S.

- NOTES:**
- RESIDENTIAL DRIVEWAYS SHALL NOT SERVE MORE THAN 3 RESIDENTIAL DWELLINGS OR EXCEED MORE THAN 40 ADT.
 - AUXILIARY LANES, WHERE WARRANTED, IN ACCORDANCE WITH STATE ACCESS CODE.
 - MAXIMUM 3 RESIDENTIAL UNITS PER RESIDENTIAL DRIVEWAY



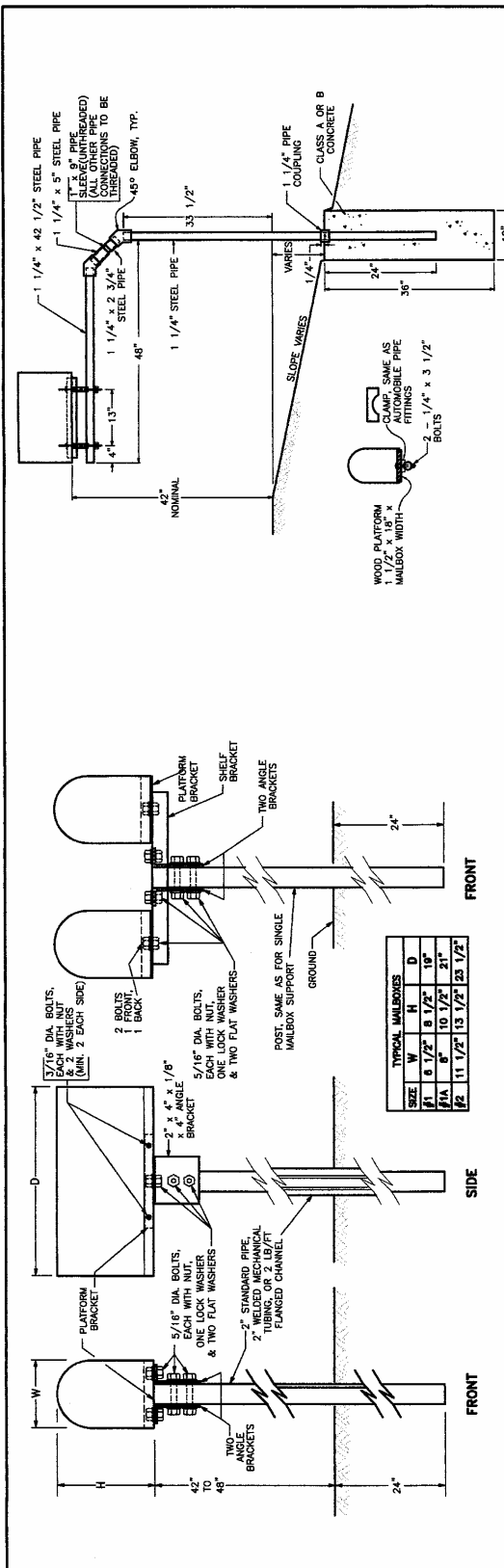
REV.	DATE	FIELD APPROACH DRIVEWAYS
1	8/06	
2		
3		
4		
5		



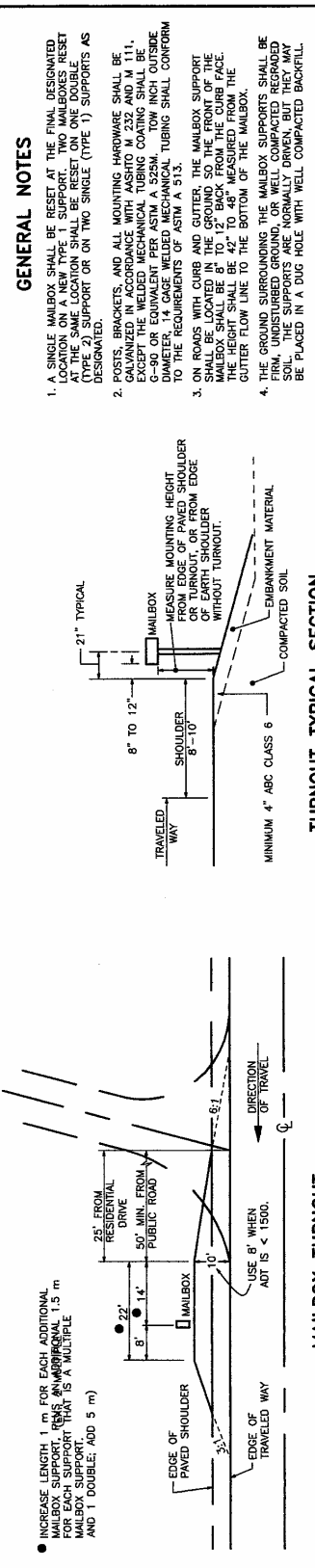
FILE NAME:	CRS-BAC1.DWG
SCALE:	N.T.S.
DRAWN BY:	ROD
CHECKED BY:	BWW

NOTES:

- FIELD APPROACH DRIVEWAY SHALL ACCESS VACANT LAND, UNDEVELOPED AGRICULTURAL PROPERTY OR ISOLATED INTERMITTENT USE STRUCTURE.
- FIELD APPROACH DRIVEWAYS SHALL NOT EXCEED ANY AVERAGE ANNUAL ADT OF 1 VEHICLE PER DAY.

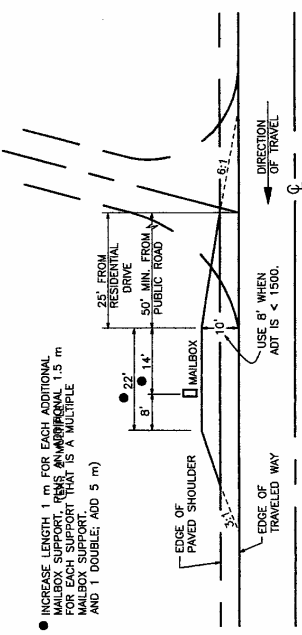


CANTILEVER (TYPE 4) MAILBOX SUPPORT



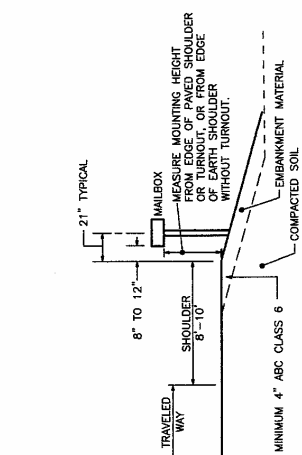
SINGLE (TYPE 1) AND DOUBLE (TYPE 2) MAILBOX SUPPORTS

- INCREASE LENGTH 1 m. FOR EACH ADDITIONAL MAILBOX SUPPORT. (SEE AN APPROXIMAL 1.5 m MAILBOX LENGTH.)
- INCREASE LENGTH 1 m. FOR EACH ADDITIONAL MAILBOX SUPPORT. (SEE AN APPROXIMAL 1.5 m MAILBOX LENGTH.)
- INCREASE LENGTH 1 m. FOR EACH ADDITIONAL MAILBOX SUPPORT. (SEE AN APPROXIMAL 1.5 m MAILBOX LENGTH.)



MAILBOX TURNOUT

NOTE: NO MAILBOX STRUCTURE SHALL BE CONSTRUCTED OF RIGID MACHINERY, MASONRY OR OTHER SUCH OBJECTS OR MATERIALS THAT WOULD CONSTITUTE A HAZARD TO THE TRAVELING PUBLIC WITHOUT TURNOUT.

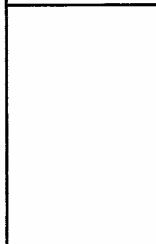


TURNOUT TYPICAL SECTION

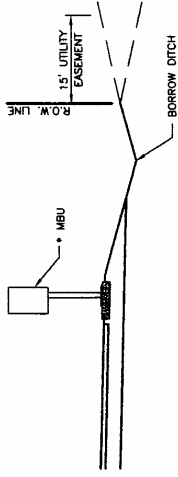
- GENERAL NOTES**
1. A SINGLE MAILBOX SHALL BE RESET AT THE FINAL DESIGNATED LOCATION AND A DOUBLE MAILBOX SHALL BE RESET AT THE SAME LOCATION SHALL BE RESET ON ONE DOUBLE (TYPE 2) SUPPORT OR ON TWO SINGLE (TYPE 1) SUPPORTS AS DESIGNATED.
 2. POSTS, BRACKETS, AND ALL MOUNTING HARDWARE SHALL BE GALVANIZED IN ACCORDANCE WITH AISHITO M 232 AND M 111, EXCEPT THE WELDED MECHANICAL TUBING WHICH SHALL BE 14 GAUGE WELDED MECHANICAL TUBING CONFORM TO THE REQUIREMENTS OF ASTM A 513.
 3. ON ROADS WITH CURB AND GUTTER, THE MAILBOX SUPPORT SHALL BE LOCATED IN THE GROUND SO THE FRONT OF THE MAILBOX SHALL BE 8" TO 12" BACK FROM THE CURB FACE. THE MAILBOX SHALL BE 8" TO 12" BACK FROM THE GUTTER FLOW LINE TO THE BOTTOM OF THE MAILBOX.
 4. THE GROUND SURROUNDING THE MAILBOX SUPPORTS SHALL BE FIRM, UNDISTURBED GROUND OR WELL COMPACTED REGRADED SOIL. THE SUPPORTS ARE NORMALLY DRIVEN, BUT THEY MAY BE PLACED IN A DUG HOLE WITH WELL COMPACTED BACKFILL.

REV.	DATE	NEW INDIVIDUAL MAILBOX
1		
2		
3		
4		
5		

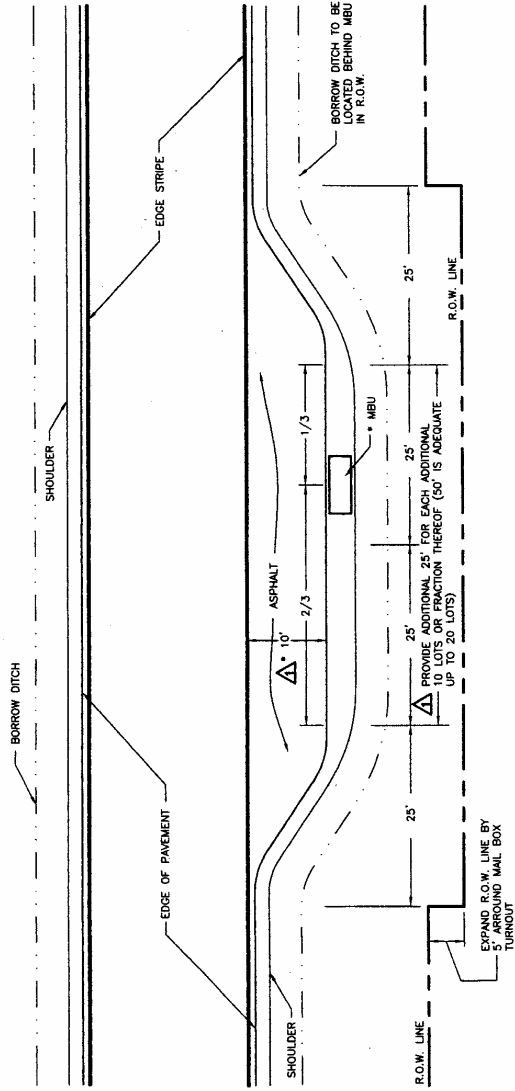
REV.	DATE	DETAILS
1		
2		
3		
4		
5		



FILE NAME	MAINT/DWG
RODD	SOLE
BIW	N.T.S.



TURNOUT SECTION



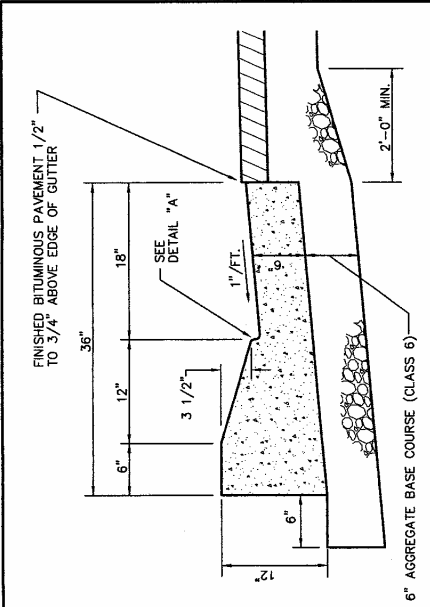
* MBU = MULTIPLE BOX UNIT FROM U.S.P.S.

MULTIPLE BOX UNIT TURNOUT

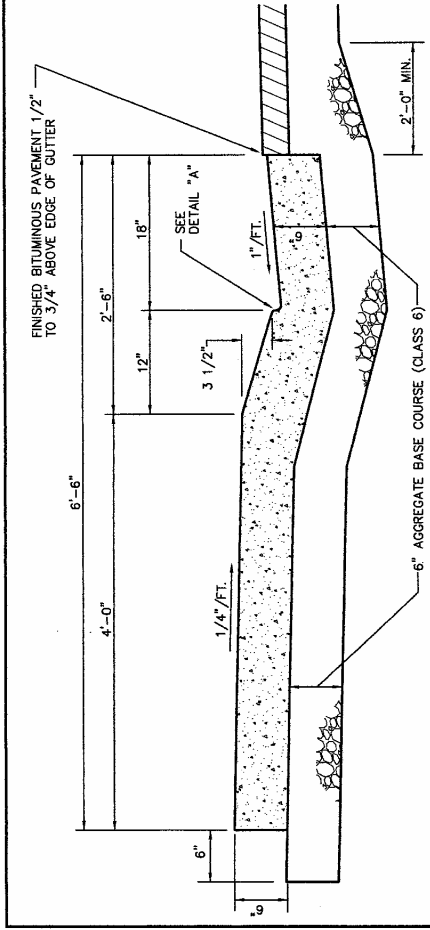
△ * 6' FOR AN URBAN ROAD SECTION WITH CONCRETE CURB AND GUTTER AND 5.5' PAVED SHOULDERS

REV.	DATE	MULTIPLE BOX UNIT TURN OUT DETAIL		
1	8/06	FILE NAME	RDD	MAINT/DWG
2		DRAWN BY:	BWW	SCALE:
3		CHECKED BY:		N.T.S.
4				
5				

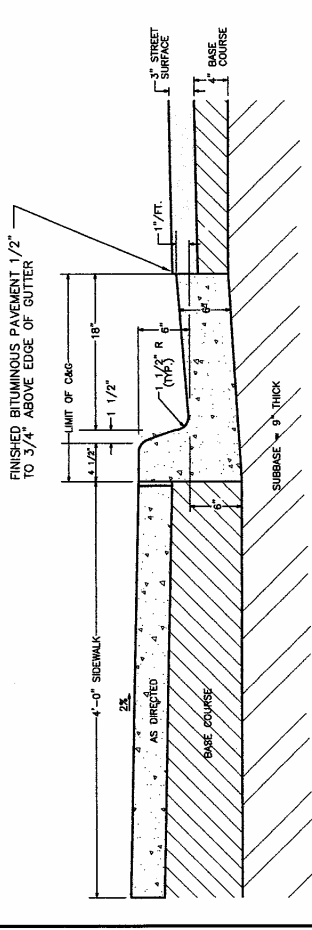
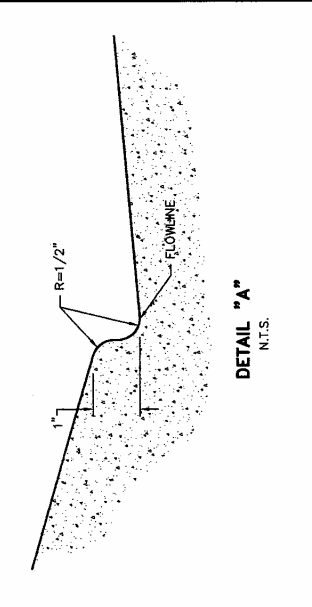




DRIVE-OVER CURB AND GUTTER
N.T.S.

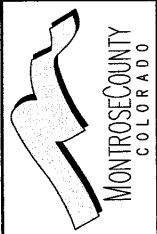


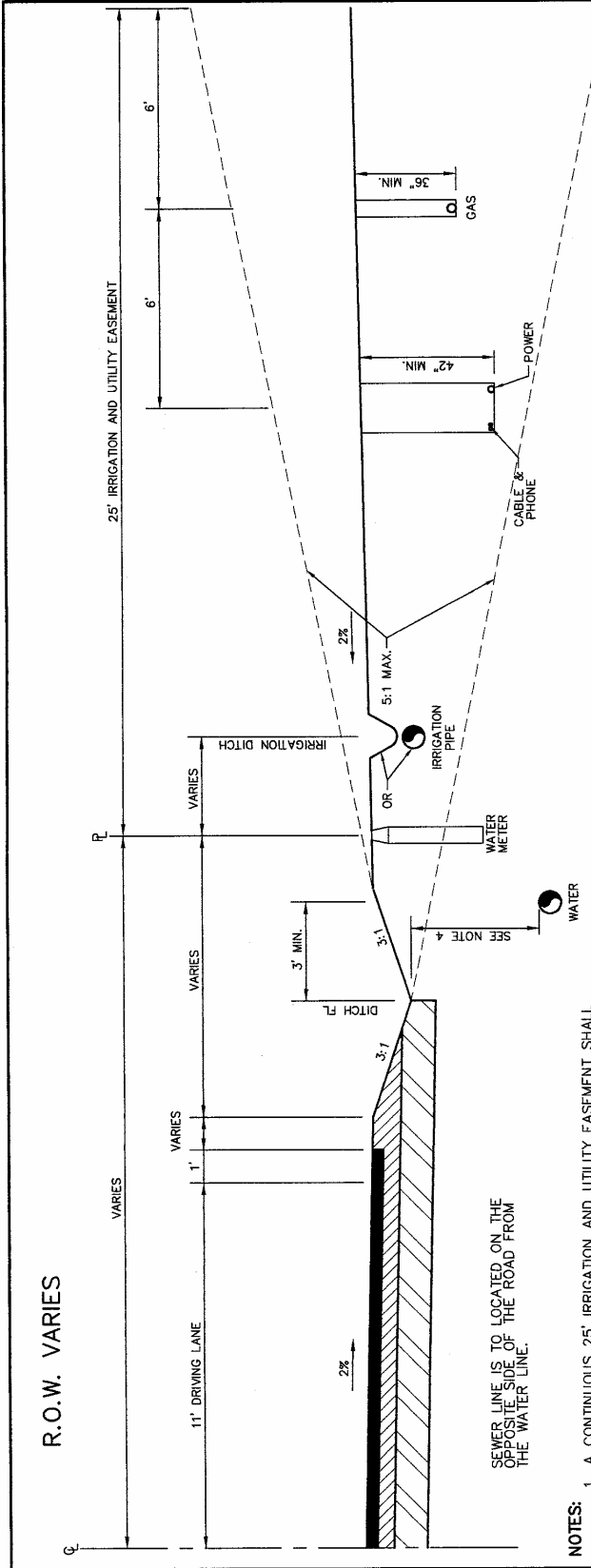
DRIVE-OVER CURB, GUTTER AND SIDEWALK
N.T.S.



VERTICAL CURB, GUTTER AND SIDEWALK
N.T.S.

REV.		DATE		CURB AND GUTTER	
1				DETAILS	
2				DRAWN BY:	FILE NAME:
3				RDD	CURBED/DWG
4				CHECKED BY:	SCALE:
5				BWW	N.T.S.





R.O.W. VARIES

25' IRRIGATION AND UTILITY EASEMENT

11' DRIVING LANE


2%

SEWER LINE IS TO LOCATED ON THE OPPOSITE SIDE OF THE ROAD FROM THE WATER LINE.

NOTES:

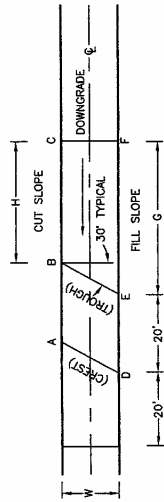
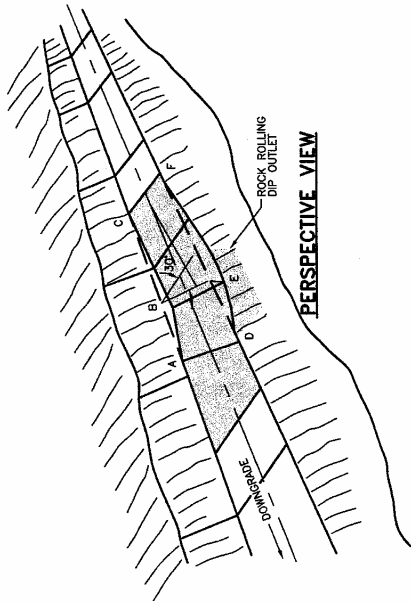
1. A CONTINUOUS 25' IRRIGATION AND UTILITY EASEMENT SHALL BE PROVIDED ON BOTH SIDES OF THE ROAD RIGHTS-OF-WAY.
2. UTILITY COMPANIES AND/OR THE COUNTY OF MONTROSE SHALL NOT BE RESPONSIBLE FOR DAMAGE TO PLANTINGS, IRRIGATION SYSTEMS, FENCES OR OTHER APPURTENANCES LOCATED OR CONSTRUCTED WITHIN THE UTILITY EASEMENT WHEN SUCH DAMAGE RESULTS FROM INSTALLATION AND/OR MAINTENANCE OF UTILITIES WITHIN SAID UTILITY EASEMENT.
3. SEWER COLLECTION SYSTEMS WILL BE INSTALLED IN THE RIGHTS-OF-WAY AS DIRECTED BY THE COUNTY ENGINEER.
4. MINIMUM COVER OVER TOP OF WATER PIPE:

TRI COUNTY	54"
MOKENA WATER	48"
CHIPETA WATER	48"

		UTILITY LOCATION & EASEMENT GUIDELINES FOR ALL STREETS	
REV.	DATE	DRAWN BY:	FILE NAME:
1		ROD	UTILD.LDWG
2		CHECKED BY:	SCALE:
3		BWW	N.T.S.
4			
5			

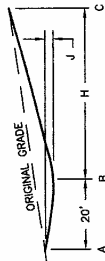
30° ROLLING DIP

ROAD GRADE IN %	W 12'			W 14'			W 16'			W 24'		
	LENGTH	DEPTH	LENGTH	DEPTH	LENGTH	DEPTH	LENGTH	DEPTH	LENGTH	DEPTH	LENGTH	DEPTH
<6	G	H	I	J	G	H	I	J	G	H	I	J
6	57	30	0.80	0.30	58	30	0.80	0.30	59	50	0.95	0.30
8	62	35	0.80	0.30	63	35	0.80	0.30	64	55	1.00	0.30
10	67	40	0.80	0.30	68	40	0.80	0.30	69	60	1.05	0.30
12	72	45	0.80	0.30	73	45	0.80	0.30	74	70	1.10	0.30
14	77	50	0.80	0.30	78	50	0.80	0.30	79	80	1.15	0.30
16	82	55	0.80	0.30	83	55	0.80	0.30	84	90	1.20	0.30

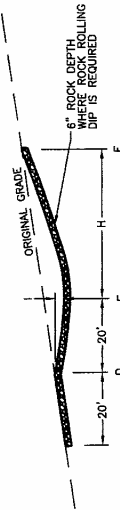


PLAN VIEW

12' ROAD 14' ROAD
 A-B = 0.3' 0.3'
 B-E = 0.5' OR 2% UPHILL SLOPE
 D-E = 0.8' 0.9-1.0'
 A-D = FLAT (+/-0.1')




PROFILE A-B-C



PROFILE D-E-F

NOTES:

- DETAILS SHOWN ARE FOR AN OUTSLOPE, INSLOPE OR FLAT ROLLING WITHOUT A DITCH. DETAILS FOR INSLOPE AND OUTSLOPE WITH A DITCH ARE SHOWN. THE DITCH IS TO BE CONSTRUCTED TO FULL ROLLING DIP DEPTH BETWEEN POINTS "B" AND "C". RESUME DITCH CONSTRUCTION 20' DOWNGRADE FROM POINT "A".
- ROLLING DIP OUTLETS WILL BE CONSTRUCTED TO DRAIN FREELY AWAY FROM ROAD. ANY OUTLET DITCHING REQUIRED TO ACCOMPLISH THIS WILL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OF THE DIP UNLESS OTHERWISE SPECIFIED.
- TOLERANCE CLASS THROUGH THE ROLLING DIP SHALL BE CLASS A (203.17).
- SLOPE OF THE ROLLING DIP FROM POINT "B" TO THE OUTLET SHALL BE EQUAL TO THE ROAD GRADE OR A MINIMUM OF 0.5" DROP (WHICHEVER IS GREATER).
- DIMENSION "G" OR "H" SHALL BE A MINIMUM OF 50' FOR ROAD GRADES UP TO 8%. WITH AN ADDITIONAL 5' OF LENGTH FOR EACH 1% OF ROAD GRADE GREATER THAN 8%.
- EXCESS EXCAVATION MATERIAL FROM ROLLING DIP CONSTRUCTION MAY BE INCORPORATED INTO THE EMBANKMENT OF THE ROAD OR USED AS FILL. CONSIDER THE EMBANKMENT OF THE ROAD IF CREST IS CONSTRUCTED. TRANSITION ON THE DOWNGRADE SIDE SHALL EQUAL TRANSITION LENGTH "H", TOP OF CREST TO ORIGINAL GRADE.
- ROCK FOR ROCK ROLLING DIP SHALL BE OBTAINED FROM COUNTY FURNISHED SOURCE. ROCK SHALL BE CRUSHED AND WELL GRACED WITH A MAXIMUM PARTICLE SIZE OF 3".
- COUNTY SHALL BE REQUIRED TO STAKE ALL ROLLING DIPS AND OTHER DRAINAGE FACILITIES.



MONTRSE COUNTY
COLORADO

REV.	DATE	ROLLING DIP	
1		DETAILS	
2			
3		DRAWN BY:	FILE NAME: MAILDLWG
4		CHECKED BY:	SCALE:
5		BHW	N.T.S.



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Resolution # 74-2005

**Resolution of
The Board of Montrose County Commissioners**

**Concerning: Establishment of a Street Sign Policy in
Conformance with the Manual on Uniform Traffic Control Devices (MUTCD)**

- Whereas**, Montrose County is experiencing rapid growth with many new residents and visitors; and
- Whereas**, Many residents are unfamiliar with the locations of county roads, or are older with vision impairments, and
- Whereas**, Existing four inch and six inch street signs are hard to see at night due to poor reflectivity and size, and
- Whereas**, Montrose County has responsibility to regulate new signage, regarding the size, style, and location, and
- Whereas**, Montrose County seeks to obtain uniformity in street signs, and
- Whereas**, The Manual on Uniform Traffic Control Devices recommends the use of capital and lower case lettering and larger signs for better recognition and safety, and
- Whereas**, Knowing which county subdivision roads are maintained or not presents difficulty to maintenance crews;
- Whereas**, the Montrose County Street Sign Policy, a copy of which is attached hereto, adequately and effectively addresses the foregoing concerns and goals.

Now, Therefore, Be It Resolved, by the Board of County Commissioners of Montrose County, Colorado that Montrose County hereby adopts and establishes as its official policy the Montrose County Street Sign Policy attached to this resolution and incorporated herein to ensure that it's citizens can find their destinations easily and safely whether in daylight or nighttime hours, and know which roads are maintained or not.

Adopted this 7th day of November, 2005.

By and Through its Board of County Commissioners
Montrose County, Colorado

Allan J. Belt
Allan J. Belt, Chairman



Attest:

Francine Tipton-Long
Francine Tipton-Long
Deputy Clerk of the Board



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Montrose County Street Signage Policy

Guide Signs - General

The purpose of guide signs is to act as navigational references for drivers as they seek their destinations. The Manual on Uniform Traffic Control Devices (MUTCD) shall be the basis for County signage unless otherwise excepted by the County Engineer.

Guide signs may be stand alone or mounted on top of stop signs if practical.

Guide Sign Location

Road name signs (street signs) in Montrose County shall be placed at the intersection of all county roadways whether maintained or not. Signs shall be parallel to the road it identifies. At intersections the intersecting street sign shall be placed 90 degrees to the intersected road.

In remote areas fiberglass composite posts (such as Carsonite Curve-Flex) will be placed in the appropriate right of way marking the road being entered on to.

Guide Sign Specifications

New street sign installations shall be comprised of 9 inch tall 0.08 gauge or thicker aluminum blank with Hi Intensity Prismatic Retro-Reflective sheeting attached to a 1-3/4 inch 14 gauge square post (minimum). If double faced street signs are needed then 2 single faced blades will be used and bolted together at the post and outside edges. The outside bolted edges will incorporate spacers between the blades for stability. Additional bracing may be used if necessary.

Lettering of street names shall be a combination of upper and lower case Hi Intensity Prismatic Retro-Reflective Sheeting or better. The upper case letter (capital) and the lower case letters shall be 6 inches high. The supplementary lettering to indicate Rd, Ct, Wy, etc. shall be 3 inch letters in the same format. If a directional letter is needed then it shall be a 3 inch high capital letter.

Fiberglass composite post installations will not require the above standard, but will consist of a 4 inch green flexible post, i.e. Carsonite Curve-Flex post with white lettering. The top of the post shall be a minimum of 4 feet from a line level with the road surface. Lettering shall be 4 inches high running vertically with 1 inch spacing between letters, and a 6 inch space before the suffix. All letters shall be centered on the post. The County Logo shall be placed 1 inch from the top of the post and the lettering shall commence 1 inch below the logo.



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New Subdivision Installation

All new approved subdivisions must acquire signage from the Road & Bridge Dept. Payment for signs and installation will be in accordance with the schedule of fees as adopted by the Board of County Commissioners on October 17, 2005 with Resolution 71-2005, or as such resolution may be amended from time to time. Signs shall be installed in a timely manner so that the final approval of the subdivision is not delayed. All signs will be placed on square tubing break away posts in conformance with the Manual on Uniform Traffic Control Devices (MUTCD).

Abbreviations

Following are the acceptable abbreviations to be used on street signs.

Standard

Abbreviation	Word Message
N	North
S	South
E	East
W	West
Cir	Circle
Ct	Court
Dr	Drive
Ln	Lane
Rd	Road
Tr	Trail
Wy	Way

Sign Color and Maintenance

Green blade with white border and lettering. County logo in the upper left side of blade.
County Road or Public Access with County Maintenance
Listed as eligible for funding on the HUTF System.

Green fiberglass post with white lettering. County logo on top.
County Road or Public Access with County Maintenance
Listed as eligible for funding on the HUTF System.

Blue blade with white border and lettering. County logo in upper left side of blade.
No County Maintenance Responsibility. (Post June 3, 1996 subdivision roads)
County will administer road permitting and addressing.
Roads may be listed on HUTF but not set up for maintenance funding.

Blue blade with white border and lettering. No County logo.
Private road and may be designated as such by homeowners on a separate sign.
All private roads will be designated as Trails.
County will maintain addressing of private roads.



Naming Conventions

All new road names will be reviewed by the GIS Department for conformance with the Montrose County Road Naming Convention and Addressing Policy. Only after approval will new signs be made. The County will adhere to the following naming guidelines where ever possible, as per policy.

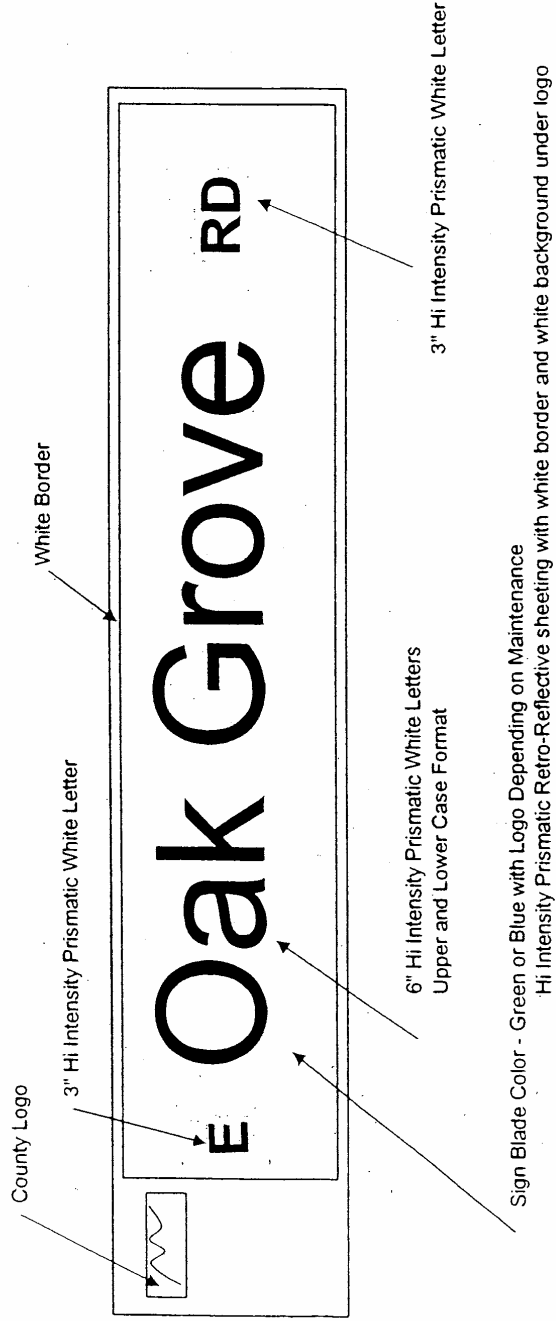
The suffix for the road name is assigned in accordance with the following:

- Trail A private non-exclusive easement serving four or more unsubdivided parcels or tracts that may or may not be built to County Road Construction Standards.
- Road A public road located in public right-of-way or a prescriptive easement serving subdivided and/or unsubdivided lands. Typically, a "road" is external to a subdivision, but may be internal to a subdivision when it will form a connecting link with other neighboring properties, and that is built to County Road Construction Standards.
- Drive A public road located in a public right-of-way internal to a subdivision that meanders (i.e. is generally oriented both N/S and E/W), and that is built to County Road Construction Standards.
- Way A public road located in a public right-of-way internal to a subdivision that provides a connecting link between other public roads within a subdivision, and that is built to County Road Construction Standards.
- Circle A public road located in a public right-of-way internal to a subdivision that meanders (i.e. is generally oriented both N/S and E/W), that intersects the same "road" at two separate locations, and that is built to County Road Construction Standards.
- Court A public road not exceeding 500 feet in length located in a public right-of-way internal to a subdivision, that is generally oriented N/S or E/W, that terminates in a permanent cul-de-sac or other approved turn-a-round configuration, and that is built to County Road Construction Standards.
- Lane A public road exceeding 500 feet in length located in a public right-of-way internal to a subdivision that is generally oriented N/S or E/W, that terminates in a permanent cul-de-sac or other approved turn-a-round configuration, and that is built to County Road Construction Standards.

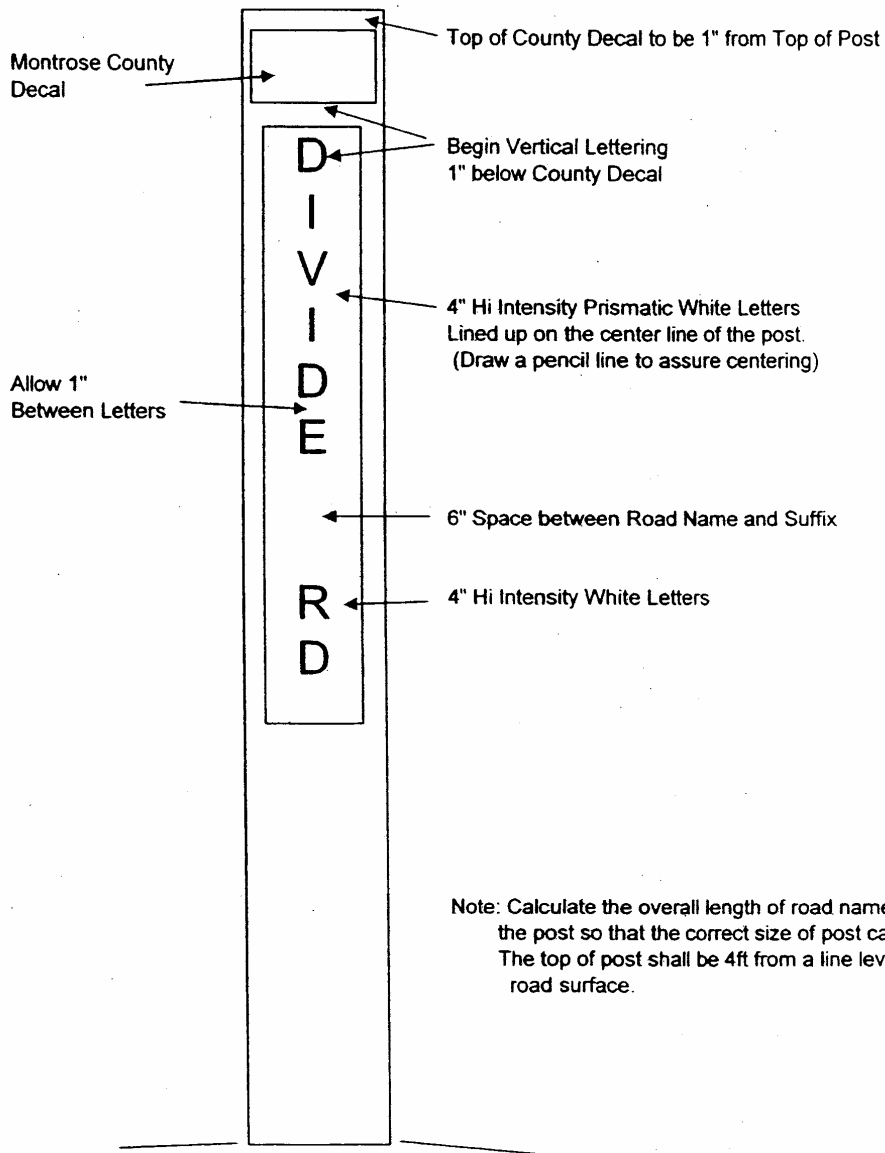
No suffixes other than those listed above shall be assigned to a new road in unincorporated Montrose County. Suffixes such as Avenue, Boulevard, Street, Parkway, Place, Run, Chase, Loop, etc. will not be considered for new roads.

Refer to the Montrose County Road Naming Convention and Addressing Policy for a more detailed explanation of how roads are named and addressed.

Montrose County Street Sign Layout



Carsonite Post Layout



Revised 10/19/05



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